



THE PLANNINGHUB

by Hawes & Swans

STATEMENT OF ENVIRONMENTAL EFFECTS

Construction of a 4-Storey Boarding House & associated works at

3 William Street, Fairfield

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1.0 Introduction

This Statement of Environmental Effects (SEE) accompanies a **Development Application (DA)** submitted to Fairfield City Council on behalf of H & F Building Pty Ltd. The DA proposes repair and restoration works to an existing fire station and the construction of a 4-storey boarding house development at 3 William Street, Fairfield.

Under this current Development Application, consent is sought for the following building works:

- Excavation works to a depth of 3m to accommodate a 1 level basement;
- Repair and restoration works to the existing heritage item (including part demolition of recent additions);
- Use of the heritage item as a commercial premise;
- Construction 30 boarding rooms comprising a mix of single and double rooms, a manager room, ground floor communal rooms and communal open spaces over 4 storeys with a maximum building height of 14.1m;
- Use of the boarding house under the provisions of the State Environmental Planning Policy (Affordable Rental Housing) 2009;
- One basement carparking is proposed, comprising a total of 8 car spaces (including 2 accessible car spaces), 6 motorcycle spaces and 9 bicycle spaces;
- Vehicular access from the laneway to the west of the site to the basement car parking servicing the development; and
- Drainage, tree removal, landscaping and associate site works.

This SEE provides a detailed description of the site and the amended proposal and provides an assessment of the proposal against the relevant heads of consideration set out in Section 4.15 of the *Environmental Planning and Assessment Act, 1979* (EP&A Act). That assessment has found that the proposal:

- Meets the current objectives of the relevant SEPP, LEP and DCP where applicable.
- Will not result in significant negative impacts on surrounding land uses and environment.
- Is responsive to site context and presents a positive visual relationship with surrounding uses.
- Is strongly in the public interest.

The development, as proposed, is considered to exhibit a high-quality design by providing a high standard of amenity to the residential apartments and will not detract from the approved building within the streetscape, and its prominent street position within Fairfield City Centre.

It also ensures the upkeep and maintenance of a local heritage item which will preserve its existence into the future.

The SEE forms part of a suite of documents that are submitted in support of the application attached as **Appendices A-T**.

1.1 Project Context

The subject site is located on the southern end of William Street in close proximity to the intersection of Hamilton Road and The Crescent, Fairfield. The site is located within land identified as being within the Fairfield City Centre and is zoned B4: Mixed Use under the provisions of Fairfield Local Environmental Plan 2013. This site is envisaged for future mixed-use residential development under the provisions of Fairfield Development Control Plan 2013.

The subject site comprises two (2) individual sites known as Lot 3, DP 3035 and Lot 1, DP 30806 and the overall intent is to consolidate those lots to allow for a holistic design approach that allows for the creation of a built form that will help upgrade and preserve the local heritage item within the site and encourage the development of the Fairfield CBD precinct in accordance with the LEP and DCP.

The proposal will align with the principles of urban consolidation to ensure the efficient redevelopment of the site by providing a mixed-use development in a strategic location, noting the proximity of the site to the Fairfield CBD itself, a range of public open space areas, and bus/train interchange.

Design consideration has also been given to broader amenity issues including solar access, views, location of open space, pedestrian movements, etc. Importantly, design consideration has been given to ensuring the existing heritage item remains a significant feature on the site and the new built form results in a positive urban design outcome that will result in active streetscapes with buildings addressing both William Street and the laneway to the west.

The key aims of the project can be summarised as follows:

1. Undertake repairs and restoration works to the existing local heritage item on site, the Fairfield Fire Station to ensure its viability into the future.
2. Provision of a development consistent with the objectives of the State Environmental Planning Policy (Affordable Rental Housing) 2009.
3. Ensure that a high level of residential amenity and diversity in housing form is allowed for and achieved within the site.
4. Provision of communal open space within the site to encourage the creation of a connected community through a modern and innovative space located at key point within the building.
5. To provide a development that is within the bulk, scale and design of development envisaged within the Fairfield Town Centre.
6. Provide required revenue to ensure the ongoing upgrade and upkeep of the existing heritage item, Fairfield fire station and allow public access to that item for all to enjoy.

1.2 Project Team

The preparation of the DA has been a collaborative effort by a team of consultants identified in Table 1 below. This has allowed for a holistic design approach that ensures the ability to deliver a development that responds to the unique physical characteristics of the site including its topography, orientation, pedestrian and vehicular access, envisaged land uses, heritage item and relationship to surrounding properties and areas.

Table 1: The Project Team	
Discipline	Consultant
Architect	Mode Architects
Town Planning Consultancy	The Planning Hub
Landscape Architect	Mode
Surveyor	GGeo Surv (NSW)
Hydraulic Engineers	Greenview Consulting
Archaeological Assessment	Heritage 21
Traffic Consultant	Greenview Consulting
Contamination Assessment	Senversa
Heritage Report & Conservation Schedule	Heritage 21
Access Assessment	Metro Building Consultancy
BCA Assessment	Metro Building Consultancy
Waste Management Plan	Dickens Solutions
QS Report	Blue Stone Management
BASIX Certificate	Building Sustainability Assessments
Acoustic Report	Acoustic Logic

1.3 Application Consultation - Issues Raised

A Development Advisory Meeting (DAM) was held with Council on 17 April 2018. The matters discussed at the meeting and documented in the formal response from Council have been taken into consideration in the formulation of the Development Application and are addressed as follows:

Table 2: Development Advisory Meeting (DAM) Requirements	
Requirement	Response
2. Clause 4.6 Statement	The proposed boarding house has a maximum height of 14.1m and therefore exceeds the statutory height limit for the zone. As suggested in Council's letter, a Clause 4.6 statement has been prepared to accompany this Development Application and is attached as Appendix D to this report.
3. Heritage Report	The proposal has been carefully planned and designed to ensure the heritage significance of the site is maintained. A Heritage Report prepared by Heritage 21 (refer to Appendix M) has been submitted with this development application and is discussed further in this report.
4. Character Statement	The proposal is considered to be consistent with the desired character of the area. A Character Statement addressing the bulk and scale of the development in comparison to the surrounding area has been included in Section 3.3 of this statement.
5. A detailed report shall be submitted detailing how the curtilage of the proposed boarding house and existing heritage item will be planned and utilised to retain the heritage item.	Details of the reduce curtilage is detailed in the Heritage Impact Assessment prepared by Heritage 21. Refer to Appendix M for further details. In addition, a schedule of conservation works has also been provided detailing the upgrade and upkeep works required to ensure the viability of the heritage item. Refer to Appendix O .
6. Excavation Report	Based on the demolition works proposed to the heritage item (new additions only) and the location of the new built form, an Excavation Report was not deemed necessary. The excavation works will cut to a depth of 3m and are located a

	sufficient distance to ensure the structural stability of the existing fire station and any significant vegetation in respect of the proposed excavation works can be protected.
7. Traffic Management Report	A Traffic Management Report has been prepared by Greenview Consulting and submitted with this application demonstrating how the proposed development will operate internally, enable traffic flows into the laneway with vehicles entering and existing the development in a forward direction. The report concludes that no adverse traffic impacts upon the existing road network will occur as a result of the proposed development. Refer to Appendix L for further details.
8. Compliance with the State Environmental Planning Policy (Affordable Rental Housing)	A compliance table has been created and is attached as Appendix C to this report demonstrating that the proposal is fully compliant with the relevant provisions of SEPP (ARH) 2009.
9. The development shall be assessed against the provisions of State Environmental Planning Policy SEPP 65 – Design Quality of Residential Flat Development.	Clause 4, part 1 of the SEPP 65 – Design Quality of Residential Flat Development details that unless a local environmental plan states otherwise, this Policy does not apply to a boarding house or a serviced apartment to which that plan applies. Therefore, SEPP 65 does not apply to the proposed development and an assessment is not required.
10. Compliance with Council's Fairfield City Wide Development Control Plan 2013	A compliance table has been created and is attached as Appendix F to this report demonstrating compliance with the relevant controls in FCDCP 2013. The boarding house includes provision for a manager's residence on site.
11. Operational Plan of Management	An Operational Plan of Management has been prepared by Mode and submitted with this application. Refer to Appendix T .
12. Waste Management Plan	A Waste Management Plan has been prepared by Dickens Solutions and submitted with this application. Refer to Appendix I .

13. Landscape Plan	Landscape Plans have been prepared by Mode in accordance with Council's Landscape Policy and submitted with this application. Refer to Appendix G .
14. Archaeological Assessment	An Archaeological Assessment has been prepared by Heritage 21 and submitted with this application. Refer to Appendix R .
15. Statement of Environmental Effects	This SEE and attached appendices is submitted to Council with this application that provides full details of the proposal and addresses all matters for consideration.
16. Flood Risk Management Report	A Flood Risk Management Report has been prepared by Greenview Consulting and submitted with this application that addresses Chapter 11 of FCWDCP 2013. Refer to Appendix H .
18. Concept Stormwater Drainage Plan	A Concept Stormwater Drainage Plan has been prepared by Greenview Consulting and submitted with this application that is in accordance with Council's Stormwater Management Policy. Refer to Appendix H .
19. Concept Earthworks Plan	Details of earthworks proposed are contained in the stormwater plans prepared by Greenview Consulting and the architectural plans detail the anticipated volume of cut proposed and these plans are submitted with this application. Refer to Appendix A & H for further details.
20. Vehicular Access	All vehicular access to the site and all parking spaces have been designed in accordance with AS2890.1-2004 Off-street car parking and Chapter 12 of FCDP 2013.
21. Pedestrian and Vehicle Safety	Clear lines of sight are provided at the property line to ensure adequate visibility between vehicles leaving the car park and pedestrians and vehicles on the frontage road in accordance with Clause 3.2.4 of AS2890.1:2004.

22. Driveway Ramp	The proposed driveway ramp is 6.17m wide and allows for a 2.6m clearance throughout the driveway ramp to the basement car park and allows for two-way traffic.
23. Longitudinal Section	A longitudinal section of the driveway ramp has been prepared by Mode and submitted with this application that shows appropriate gradients of the proposed driveway, head clearance and in accordance with AS2890.1:2004.
24. Label Dimensions	The dimensions of all car parking spaces, aisle widths and internal road widths have been provided in the architectural plans.
25. BCA Report	A BCA Report has been prepared by Metro Building Consultancy and submitted with this application. Refer to Appendix Q for further details.
26. BCA Compliance	A BCA report has been provided which details some alternate solutions. This is detailed further in Appendix Q .
27. Documentation	All documentation outlined in Council's DAM minutes has been provided with this development application submitted to Council.

1.4 Planning Framework and Referrals

The site is located in the Fairfield Local Government Area (LGA) and as such the DA will be determined by Sydney Western City Planning Panel given the CIV for the proposed development is \$9.08m and therefore exceeds the \$5m threshold as set out in Schedule 7 of the State Environmental Planning Policy (State and Regional Development).

In preparation of the development, consideration has been given to the following planning instruments:

- Greater Metropolitan Regional Environmental Plan No 2—Georges River Catchment
- State Environmental Planning Policy (Affordable Rental Housing) 2009
- State Environmental Planning Policy No.55 – Remediation of Land
- State Environmental Planning Policy (Building Sustainability Index: BASIX)
- Fairfield Local Environmental Plan 2013
- Fairfield Town Centre Development Control Plan 2013
- Fairfield Citywide Development Control Plan 2013

It is not considered that any other external referrals will be required based on the existing site conditions and constraints.

2.0 Site Analysis

2.1 Location and Context

The site is situated within the Fairfield City Centre on land zoned B4: Mixed Use under the provisions of Fairfield Local Environmental Plan 2013 and is approximately 23km from the Sydney CBD. The surrounding area consists of the following land use types, facilities and services:

- Fairfield Community Church
- Fairfield Branch Library
- Crescent Park
- Fairfield Train Station & Bus Interchange
- Our Lady of the Rosary Catholic Primary School
- Warakirri College
- Fairfield Court House
- Fairfield Police Station
- Fairfield Nursing Home
- Fairfield Early Childhood Health Centre
- Centrelink and Medicare
- City of Fairfield RSL Club and Memorial Garden
- Fairfield Hotel
- Range of Food & Drink Premises

The site's locational context is shown in **Figures 1 and 2** below.

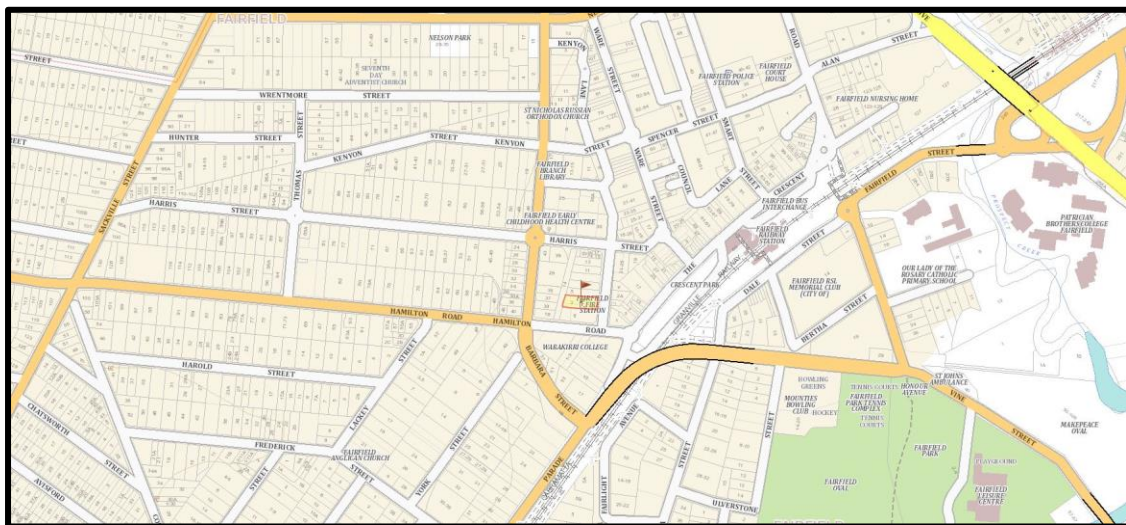


Figure 1: Site Location Plan identifying the subject site coloured red. (Source: SixMap)

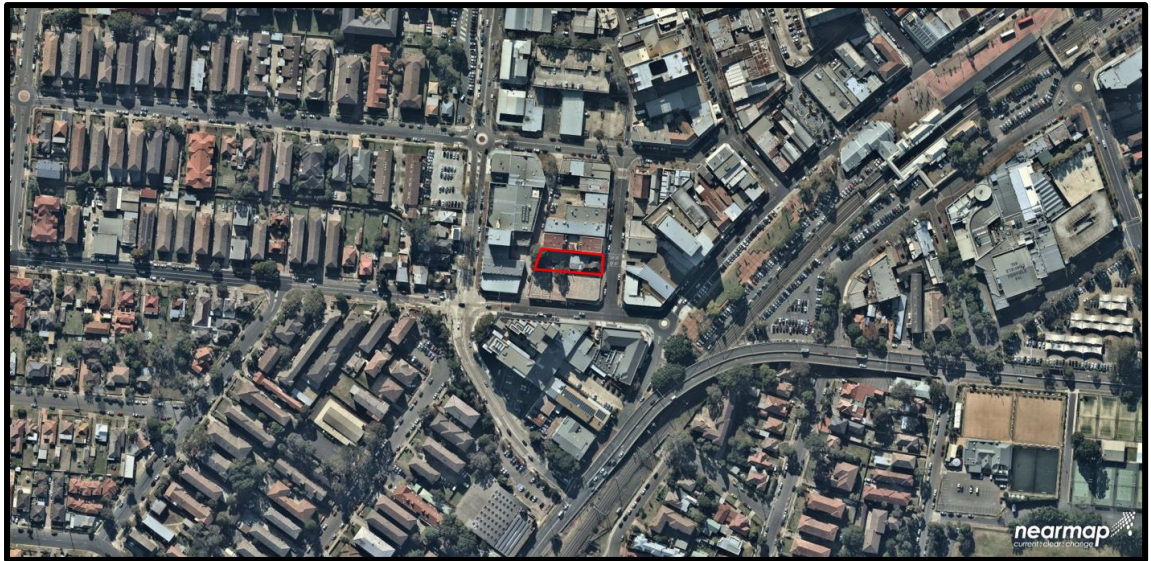


Figure 2: Site Aerial (Source – NearMap).

2.2 Site Description

The site is located at 3 William Street, Fairfield and is legally described as Lot 3, DP 3035 and Lot 1, DP 308601. The site currently contains Fairfield Fire Station sited at the William Street frontage that is a local heritage item that will be retained and upgraded as part of this proposal.

The site has a total area of 961.1m² and has a primary frontage of 18m to William Street, as well as a secondary frontage to a laneway (Harris Lane). Existing vehicular access to the site is provided from William Street.



Figure 3: Aerial Photo of the Site demonstrating the subject site outlined in red. (Source: NearMap)

2.3 Existing Site Conditions

The subject currently contains existing structures and landscaping as detailed below:

Existing Structures	The site contains an existing building (Fairfield fire Station, a local heritage item) and associated structures.
Access	The site is currently accessed via William Street through an existing driveway crossover and it also has access through a laneway (Harris Lane) on the western boundary of the site.
Vegetation	The site contains some scattered vegetation, none of which is understood to be of significance.
Easements	There are no current easements are applicable to the site as shown in the survey plan prepared by GGeo Surv (NSW) provided in Appendix B .

3.0 Proposed Development

3.1 Overview

This DA proposes the following works on land identified Lot 3, DP 3035 and Lot 1, DP 308061 (3 William Street, Fairfield):

- Consolidation of two (2) lots, legally described as Lot 3, DP 3035 and Lot 1, DP 308061;
- Demolition of recent additions to the existing heritage item on site;
- Use of existing heritage item as a commercial space.
- Removal of existing trees as detailed in the landscape plans;
- Excavation works to a depth of 3m to accommodate a 1 level basement;
- Construction 30 boarding rooms comprising a mix of single and double rooms, a manager room, ground floor communal rooms and communal open space s over 4 storeys with a maximum building height of 14.1m;
- Use of the boarding house under the provisions of the State Environmental Planning Policy (Affordable Rental Housing) 2009;
- Upgrade works to the existing heritage item as detailed in the Schedule of Conservation Works prepared by Heritage 21;
- One basement carparking is proposed, comprising a total of 8 car spaces (including 2 accessible car spaces), 6 motorcycle spaces and 9 bicycle spaces;
- Vehicular access from the laneway to the west of the site to the basement car parking servicing the development; and
- Drainage, landscaping and associate site works.

3.2 Numerical Overview

The key numeric development information is summarised in **Table 3** below.

Table 3: Numerical Overview	
Feature	Proposed
Site Area	961.1m ²
Commercial Gross Floor Area	114.81m ²
Residential Gross Floor Area	1,079.4m ²
Total Gross Floor Area	1,194.21m ²
Floor Space Ratio	1.24:1
Maximum Building Height	14.1m
Boarding Rooms	30 rooms
Boarding Room Mix	5 x single rooms 25 X double rooms
Communal Open Space	337m ²
Landscaped Area	362m ²
Residential Car Parking Spaces	7 spaces
Commercial Car Parking Spaces	1 spaces
Motorcycle Parking Spaces	6 spaces
Bicycle Spaces	9 spaces
Solar Access Compliance	100% receive at least 2 hours mid-winter between 9am-3pm
Cross Ventilation Compliance	90% (27 units across Levels 1-3)

3.3 Built Form and Design

As detailed in the architectural plans prepared by Mode, this building has been designed in accordance with the desired and envisaged character for the mixed-use area of Fairfield City Centre. A key factor in that design was the need to ensure the significance of the heritage item present on site was not adversely impacted by the built form being proposed.

The built form proposed is required to generate sufficient revenue to ensure the repair, restoration and ongoing maintenance of the heritage item into the future.

The development has been designed in response to the site analysis taken and demonstrates compatibility with the prescriptive measures to achieve the desired character of the B4 Mixed use zone as required by the Fairfield LEP and DCP despite the height variation.

Bulk and Scale

A number of design options were considered by the project team and the preferred design option arrived at is considered to be the preferred option as it continues to protect and respect the heritage significance of the heritage item whilst allowing the site to be redeveloped to meet the objectives of the zone.

It also allows for the necessary revenue to ensure maintenance and upgrade works to the heritage item can be maintained into the future. The following details that design journey:

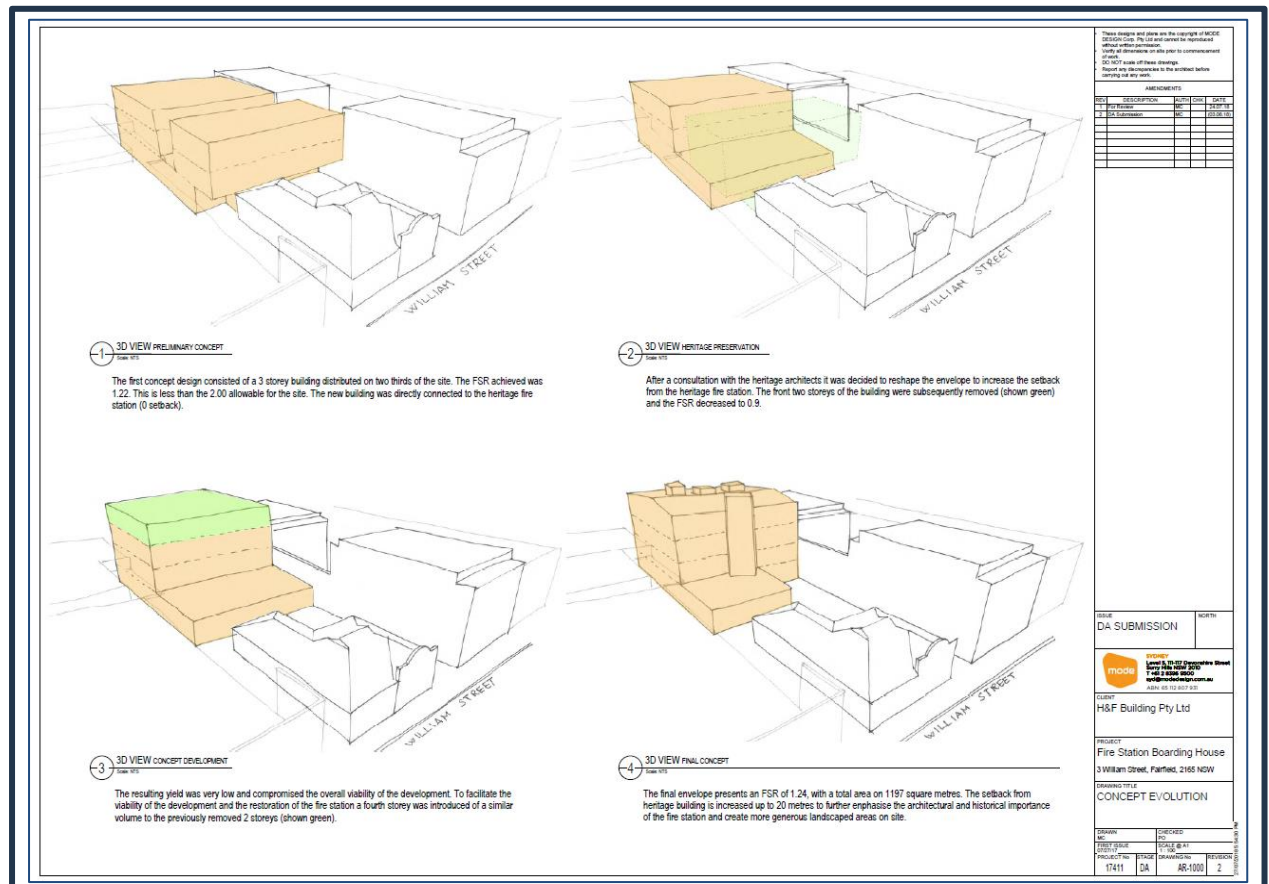


Figure 4: Concept Evolution (Source: Mode)

Design Option 1: The first concept design consisted of a 3-storey building distributed on two thirds of the site. The FSR achieved was 1.22. The new building was directly connected to the heritage fire station (0 setback).

Design Option 2: After a consultation with Heritage 21, it was decided to reshape the envelope to increase the setback from the heritage fire station. The front two storeys of the building were subsequently removed (shown green) and the FSR decreased to 0.9.

Design Option 3: The resulting yield was very low and compromised the overall viability of the development. To facilitate the viability of the development and the restoration of the fire station a fourth storey was introduced of a similar volume to the previously removed 2 storeys (shown green).

Design Option 4: The final envelope presents an FSR of 1.24 and a maximum height of 14.1m. The setback from the heritage building is increased up to 20m to further emphasize the architectural and historical importance of the fire station and create more generous landscaped areas on site.

Dwelling Diversity

The proposal accounts for the following boarding room diversity:

- 17% x single bedroom; and
- 83% x double bedroom;

The room mix will offer a variety of housing choice that responds to market demand, the bedroom numbers are varied to meet the needs of future occupants and also provide different pricing points for the alternative sizes which will contribute to affordability. In addition, 3 of the proposed double rooms are adaptable and can be made accessible if required.

Building Layout & Design

As noted in the Design Verification Statement prepared by Mode, this proposal is considered appropriate for the site and commensurate in scale and height with the current developments and desired future character of the Fairfield Town Centre and applicable B4 Mixed Use zone.

The scale of the proposed building when viewed from surrounding developments is consistent with the bulk and scale already in existence and this is detailed in the following Figures:

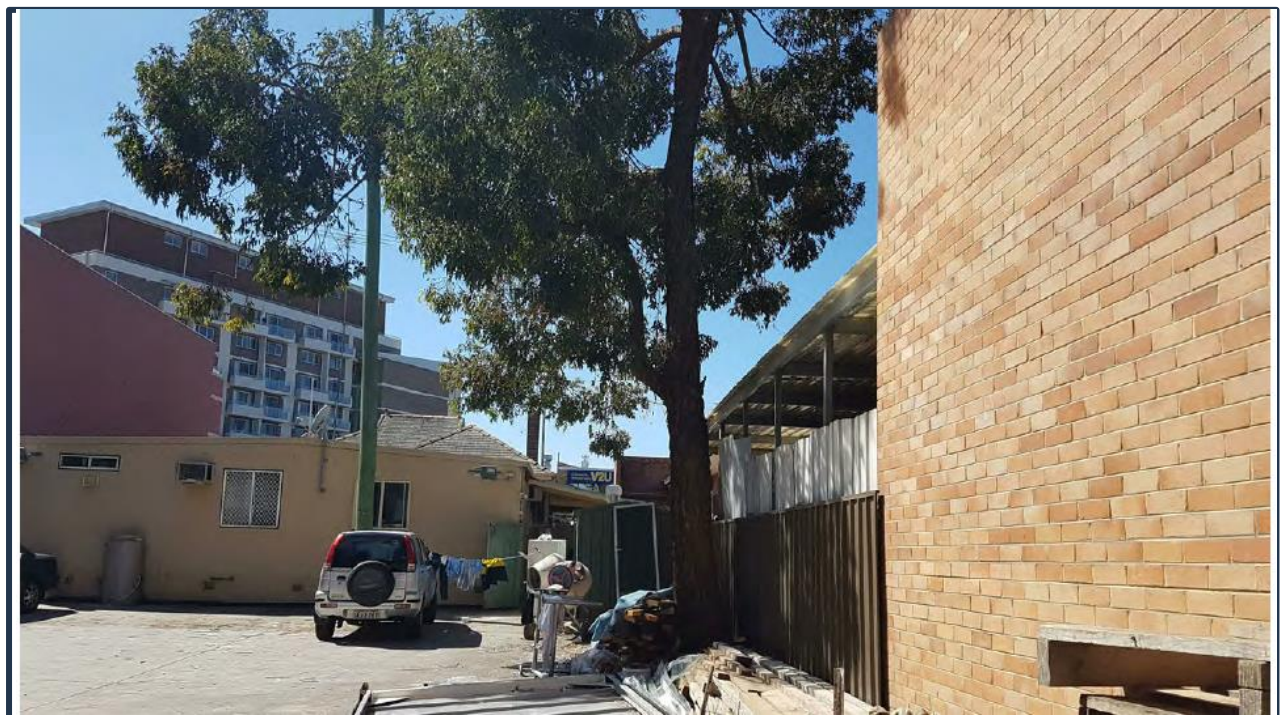


Figure 5: Western Facade and courtyard. The original Heritage building is mostly hidden by the weatherboard extension. An 8 storey residential flat building is visible in the background.



Figure 6: View from the courtyard. An Eight storey affordable apartment development is visible to the South

As detailed in the architectural plans submitted with this application (**Appendix A**), the development incorporates a variety of façade treatments.

3.4 Residential Amenity

Mode have designed the proposal to provide all boarding rooms with a high level of residential amenity and outlook. All 30 rooms proposed with the design receive the required solar access provisions and 90% proposed benefit from cross flow ventilation.

Three (3) noise sources are identified as having potential impacts upon the residential amenity of the building being noise intrusion from road traffic, mechanical plant as well as noise generated from the communal open space to windows of adjacent units.

Detail on acoustic impacts are provided in the Acoustic Report prepared by Acoustic Logic submitted in support of this DA (**Appendix M**) and discussed in **Section 4.2** of this report.

All boarding rooms will also comply with the regulatory provisions relating to sound transmission and insulation documented in the National Construction Code Series Building Code of Australia Part F5 and this will further enhance residential amenity.

Landscaping has been provided at the ground floor level and Level 1. Additional landscaping has been incorporated along the perimeter of the site where possible to act as a buffer to the adjoining properties and soften the edge of the development. Landscape planters have also been provided at the end of the corridors on residential levels to further enhance residential amenity.

All boarding rooms have been provided with private courtyards/balconies. Communal terraces are also provided on ground floor and Level 1 of the building that add visual interest to the eastern and western façades of the development, fronting William Street and Harris Lane.

In addition, amenity of adjoining developments has also been considered and shadow diagrams prepared by Mode demonstrate that adjoining properties will still maintain the required levels of solar access based on the siting, bulk and scale of the development proposed.

3.5 External Materials & External Finishes

Consideration has been given to the comments and feedback made by Council staff and a schedule of external colours, materials and finishes has been submitted with this proposal. The design incorporates robust, low maintenance materials to avoid weathering and failure of applied finishes. Furthermore, the development incorporates varying façade treatments for the ground floor and upper levels that are of a high-quality design and finish.

Materials and finishes for the upgrade works to the heritage item are detailed in the schedule of conservation works prepared by Heritage 21. Refer to **Appendix O**.



Figure 7: Proposed perspective illustrating materials on eastern facade (Source: Mode).



Figure 8: Proposed perspective illustrating materials and finishes on western facade (Source: Mode).

3.6 Pedestrian and Vehicle Access

The proposal has been designed to reflect and incorporate the following:

Pedestrian Access

- Pedestrian access to the boarding house building will be provided via Harris Lane.
- The commercial premises on site will be accessible directly from the William Street frontage.
- All boarding house rooms, the communal open space areas can be accessed from the lift core within the building.
- Lifts provide access to floors as described above from the basement levels and ground floor foyers.
- Access to the communal open space area on ground level and level 1 will be restricted to residents and visitors.

Vehicular Access

Vehicular access for residents, visitors and service vehicles is to be via a ramp down from Harris Lane to basement car parking over one (1) level. Sufficient clearance space has been provided for vehicles to enter and exit the site. Waste storage occurs at ground level and waste trucks are not required to enter the basement. Refer to the traffic and car parking analysis prepared by Greenview Consulting in **Appendix L** for further details.

3.7 Waste Management

A Waste Management Plan has been prepared by Dickens Solutions and is attached as **Appendix I** of this report. This WMP details the proposed waste management arrangements during the demolition, construction and operational phases of the development in accordance with the residential waste and recycling acceptance criteria of Fairfield City Council. Key features are detailed below:

- **Demolition & Construction Waste Management**
 - Demolition, construction and use of the proposed development will be carried out in accordance with the Waste Management Plan submitted for Construction Waste.
- **On-Going Residential Waste**
 - A designated garbage area is provided on ground level on the western portion of the site which accommodates 13 bins for general and recyclable waste streams.
 - A licensed private waste and recycling collection contractor will provide all residential waste and recycling services to the development.
 - All waste is to be placed in the red lidded waste bins. All recyclable material is to be placed in the yellow lidded recycling bins. All waste and recyclables should be appropriately bagged or wrapped prior to being deposited into the designated bin.
 - Appropriate signage will be erected within the WSA to assist the occupants placing their waste and recyclables into the appropriate bins.
 - All boarding house occupants will be responsible for depositing their waste and recycling material into the appropriate bins.
- **On-Going Commercial Waste**

- One (1) commercial tenancy will be located within the building. The tenancy will be located on the ground floor of the building with a frontage to William Street.
- The unit will be required to have commercial waste services provided in accordance with the waste generation rates of its use.
- The unit has a floor area of 115 square metres, and if used for retail purposes (excluding a food shop), it is anticipated to generate waste at a rate of 50-litres per 100 square metres of floor area per day (50 x 1.15 x 6 (assuming the unit operates 6 days per week)), which equates to 345 litres of waste per week.
- This will require the provision of 1 x 240-litre mobile waste bins serviced at least twice per week.
- All commercial waste services are to be undertaken in a manner that will not adversely impact on the principles of health, safety or convenience.

3.8 Excavation Works

This proposal involves excavation works to construct one basement levels that will not have any adverse impacts on surrounding developments. Excavation to a depth of 3m is proposed. Any new earthworks required to support this DA will comply with Council's Engineering Design Specifications and conditions of consent where relevant.

3.9 Transport, Access and Parking

As noted, the subject site is located within the Fairfield Town Centre and is situated approximately 350m to the east of Fairfield Train Station and the Fairfield Bus Interchange. It is also noted that various bus stops are present along Hamilton Road and other surrounding streets.

As discussed in **Section 3.6** above, vehicular access for residents and visitors to the boarding house is to be via a ramp down from Harris Lane to the proposed basement car parking. This has been detailed on the plans prepared by Mode. The proposal incorporates a total of 8 on site car parking spaces (including disabled parking spaces, visitor car parking and commercial car parking). All car spaces and aisle widths are fully compliant with AS-2890.

Construction traffic will be managed through the provision of B-Class hoarding which will be installed on the road frontage with access for demolition and excavation. A Works Zone would be provided on the western frontage with a site crane to unload trucks.

Further details on construction access, operational site access and parking are located in the Traffic and Parking Impact Assessment prepared by Greenview Consulting in **Appendix L**.

3.10 Heritage Works

The site is listed as a local heritage item under the Fairfield LEP 2013 (referred to as Item No. I66). It does not sit in area of heritage conservation, nor is it listed as a State Heritage item.

The development proposes to demolish a weatherboard and brick masonry extension that was added and remove some mature trees at the rear of the site and replace those with native trees in a more appropriate location. It also proposes its change of use to a commercial space.

As detailed in the statement of heritage impact statement prepared by Heritage 21 (refer to **Appendix N**), the proposed development seeks to conserve and reinstate the Fairfield Fire Station to its original condition and configuration. This involve a series of restoration works as detailed in the schedule of conservation works (refer to **Appendix O**).

It also seeks to construct a new four storey boarding house at the rear of the site which has been sited to ensure its separation from the heritage item.

That building proposes a maximum building height of 14.1m which results in a building height variation of 5.1m. The additional building height proposed is required to provide sufficient revenue to undertake the repair/maintenance works of the existing heritage item on site and has been arrived at by reducing built form adjoining the heritage building and adding it to the western portion of the site.

This is detailed in the statement of heritage impact which notes the following:

The proposal also intends to demarcate and separate the heritage item from the new development by means of a clear change in materials and a footpath leading to an entrance to the new development, thus maintaining, to a large extent, the original amenity of the heritage item. Additionally, considerable space between the heritage item and the new development has been proposed to allow curtilage around the heritage item.

The concept evolution and final plans also reflect the proposals intent to keep a low lying single storey development closer to the heritage item and create cross connecting pathways and communal spaces to ensure that the heritage item and its amenity continue to be used. A large open area is also retained along the periphery of the item to minimise negative affects to the heritage item.

Based on the above, Heritage 21 are of the opinion the proposed demolition works, use of the Fairfield Fire Station as a commercial space and the construction of the new built form (including the proposed height variation) does not and will not adversely impact in the heritage significance of the heritage item on site. Refer to **Appendix N** for further details.

4.0 Assessment of Environmental Impacts

4.1 Statutory Planning Framework and Compliance

An assessment of the proposal has been made against the relevant planning instruments applicable to the land and the proposal. The Environmental Planning Instruments that relate to the proposed development are:

- Greater Metropolitan Regional Environmental Plan No. 2 – Georges River Catchment

- State Environmental Planning Policy (Affordable Rental Housing) 2009
- State Environmental Planning Policy (Building Sustainability Index: BASIX) 2004
- Fairfield Local Environmental Plan 2013
- Fairfield Town Centre Development Control Plan 2013
- Fairfield Citywide Development Control Plan 2013

4.1.1 Greater Metropolitan Regional Environmental Plan No 2 – Georges River Catchment

The subject site is located approximately 2.8km north of George’s River. The proposed development is considered to satisfy the general planning principles of this Plan and would not impact upon activities within other local government areas adjacent to the River based on the works proposed and the distance from the river itself.

The proposal is also deemed to be compliant with the specific planning principles outlined in Clause 9 of this Plan based on the works proposed.

Furthermore, the proposed works are unlikely to significantly affect the water quality and river flows of the Georges River or its tributaries and the environment within the Catchment and therefore do not form part of any development indicated as having specific planning requirements in Part 3, Clause 11 of this Plan.

The proposed development is consistent with the aim of MREP No. 2 – Georges River Catchment and is therefore satisfactory in this regard.

4.1.2 State Environmental Planning Policy No. 55 – Remediation of Land

SEPP 55 requires Council to consider whether the subject land of any development application is contaminated. If the land requires remediation to ensure that it is made suitable for a proposed use or zoning, Council must be satisfied that the land can and will be remediated before the land is used for that purpose.

A Preliminary Contamination Assessment prepared by Senversa Pty Ltd and details the following:

- Based on the results of the PSI, Senversa developed a CSM detailing potential source, pathway receptor linkages. The CSM identified potentially complete pollutant linkages resulting from:
 - historical operations including the storage, handling, and use of petroleum products, chemicals, firefighting foams and potential hazardous substances;
 - potential impacted surface materials resulting from degradation of hazardous materials within onsite building structures; and
 - potentially impacted fill materials from unknown origin.

Based on information reviewed as part of this PSI and the completed CSM, it is the opinion of Senversa that additional investigation is required to assess the contamination status of the site.

Senversa notes that while further investigation is required to assess potential contamination within the Site, where the process of investigation, subsequent site remediation / environmental management (if required) and site audit (by an auditor accredited under the NSW Contaminated Land Management Act 1997) are undertaken in accordance with relevant NSW EPA made / endorsed guidance, it is the opinion of Senversa that the site can be made suitable for the intended development.

A copy of the report is provided in **Appendix S**.

4.1.3 State Environmental Planning Policy (Building Sustainability Index: BASIX)

The SEPP aims to encourage sustainable residential development and requires DA's for such development to be accompanied by a list of BASIX commitments as to the manner in which the development will be carried out. The proposed development is consistent with the aim of the SEPP and all of its planning controls.

A BASIX certificate has been prepared for the proposed development by Building Sustainability Assessments and demonstrates that the proposed residential flat building satisfies the requirement of the BASIX SEPP. The BASIX certificate is located at **Appendix P**.

4.1.4 Fairfield Local Environmental Plan 2013

Permissibility

The site is situated within the Fairfield Local Government Authority (LGA) and is subject to the provisions of the Fairfield Local Environmental Plan (LEP) 2013. The subject site is zoned B4 – Mixed Use under the provisions of the Fairfield Local Environmental Plan 2013 and development for the purpose of a *boarding house* is permitted with consent.

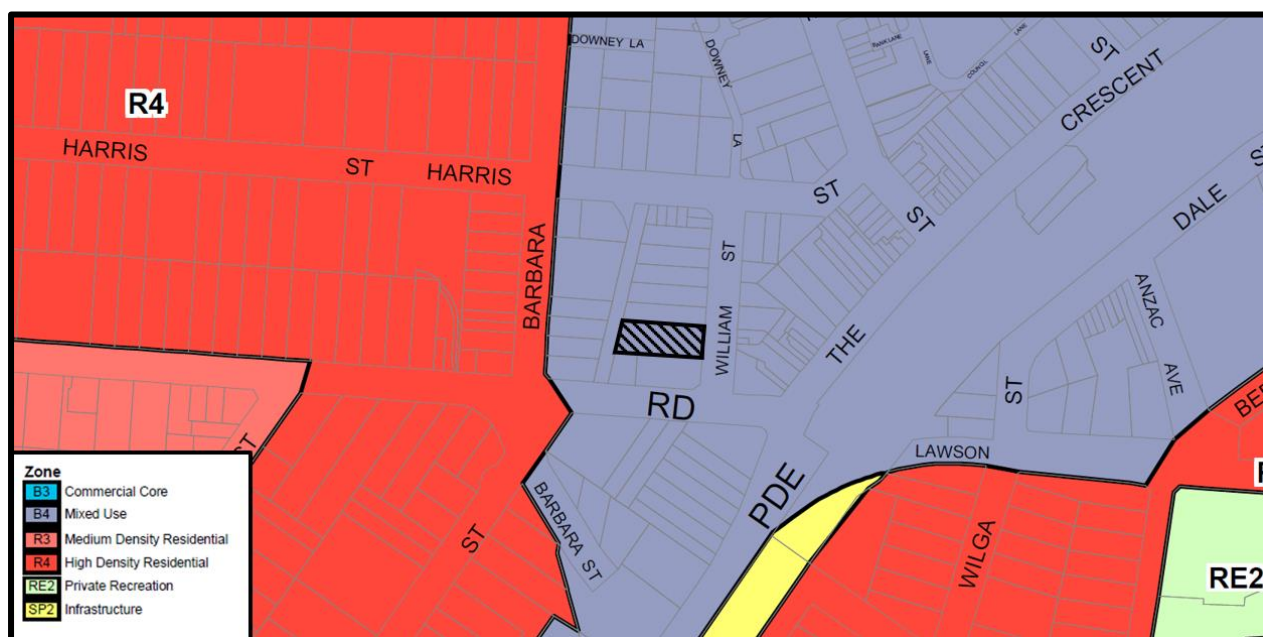


Figure 9: Land Use Zoning Map demonstrating the subject site outlined in blue.

Zone Objectives

The objectives of the B4: Mixed Use Zone are as follows:

- *To provide a mixture of compatible land uses.*
- *To integrate suitable business, office, residential, retail and other development in accessible locations so as to maximise public transport patronage and encourage walking and cycling.*
- *To support the development of Prairiewood, Fairfield and Cabramatta as the principal locations for specialist cultural, retail, business, tourist and entertainment facilities and services.*

Comment:

It is considered that the proposed development is consistent with the relevant objectives of the B4 zone in that the DA proposes a boarding house development within the Fairfield City Centre including commercial space at the ground floor level fronting William Street. The site is also located in close proximity to Fairfield Train station, bus stops and a range of other services and facilities to meet the needs of the residents.

A full response to all requirements of the Fairfield LEP 2013 is provided for at **Appendix E**. The application proposes a variation to Council's height control and Clause 4.6 statements have been provided in **Appendices D**.

4.1.5 Fairfield Development Control Plan 2013

An assessment of the proposal against the relevant development controls in the Fairfield Development Control Plan 2013 applying to the subject land is provided for at **Appendix F**.

4.2 Acoustic Impacts

An Acoustic Assessment has been prepared by Acoustic Logic (refer to **Appendix M**) and submitted with the proposal that presents an acoustical review of the proposed development. Three (3) noise sources are identified as having potential impacts upon the residential amenity of the building being noise intrusion from road traffic, mechanical plant as well as noise generated from the communal open space areas to adjoining developments. The findings and recommendations of this report are outlined below:

Conclusions & Recommendations

In order to comply with the noise emission and noise intrusion criteria as detailed in the previous sections, the following building and management controls are required:

- Up to 15 people to be using the outdoor communal area at any time.
- Communal areas (indoor or outdoor) are not to be used for parties.
- The outdoor communal area should not be used between the hours of 10pm to 7am.
- All windows of the indoor communal area should be minimum 6.38mm thick with acoustic seals around the perimeter.

- All windows of the internal communal area should be closed between the hours of 10pm to 7am.
- Construct a minimum 2.1m high solid vertical fence at the northern boundary. The fence may be constructed of lapped capped timber or metal sheet.

The recommendations contained within the acoustic report are considered to be satisfactory and will therefore ensure no adverse noise impacts occur on the subject development or adjoining development based on its operations.

4.3 Heritage Impacts

As detailed, the site comprises a local heritage item, Fairfield Fire Station, and a series of works are proposed to that item including demolition of recent additions and repair/restoration works. Those works are detailed in the schedule of conservation works as well as the heritage impact assessment prepared by Heritage 21.

Those works will see the restoration of the heritage item back to its original condition and will ensure its retention into the future. It also proposes its use as a commercial premise which will allow public access to the building for members of the public to enjoy.

In addition to the above works, the development proposes a four storey boarding house at the rear of the site. That boarding house proposes a maximum building height of 14.1m, which exceeds the existing height limit by 5.1m. The impacts of this height variation are deemed to be reasonable based on the following rationale:

- It does not generate a negative impact on the existing heritage item as outlined in the statement of heritage impact prepared by Heritage 21 (**Appendix N**);
- It allows for sufficient revenue to be generated to ensure repair/restoration and ongoing maintenance works to the Fire Station to be completed to ensure its survival into the future;
- The new development has been designed in an appropriate manner that is sensitive to the surrounding area and provides for the ongoing conservation of Fairfield Fire Station, and appreciation of its heritage significance to a wider public audience;
- A comprehensive schedule of conservation works plan has been prepared (**Appendix O**) for approval by Council as part of this application;
- A heritage impact assessment (**Appendix N**) has been prepared and notes that the proposed development will not adversely affect the heritage significance of the heritage item, including its setting.

4.4 Visual Impacts

From a visual perspective, an appropriate composition of building elements, material textures and colours have been implemented in the design scheme to reflect the character of the building and its intended residential use. The articulation of the external facades of the reflects the desired future character of the Fairfield City Centre.

The future context of the area has been taken into account and the proposed development has been designed with various architectural elements to provide articulation, depth, shade and a pleasing aesthetic. The building design has been articulated significantly enhance the public domain and provide visual interest within the streetscape and rear laneway.

The proposal is considered to represent a positive contribution to the streetscape and its design and location of car parking within basement levels ensures the amenity of adjoining residents is not unduly compromised. Refer to perspectives created by Mode in **Appendix A** for further details.

4.5 Stormwater and Drainage

In accordance with Council's guidelines for stormwater and water cycle management, Greenview Consulting have developed a conceptual stormwater management plan incorporating reuse and discharge. The stormwater concept envisages an OSD tank within the basement level which will collect stormwater runoff from a series of connected pits and pipes.

From here, the stormwater will be dispersed to an on-site water quality device before being discharged to a single connection point within the public drainage network.

The site is identified as being situated within a flood prone area. Water quality treatment is proposed to be achieved in accordance with Council's Engineering Specifications. Refer to **Appendix H** for further details.

4.6 Infrastructure and Services

Essential services will be provided as follows:

Potable Water Supply	It is intended to supply potable water to the site from the existing mains.
Waste Water	The development will be connected to the existing sewer system.
Electrical Supply	Electricity supply will be provided by extension of the existing mains.
Telecommunications	Telecommunications to the site will be provided by extension of the existing service.

4.7 Construction Management

Construction traffic will be managed through the provision of B-Class hoarding which will be installed on the road frontage with access for demolition and excavation. A Works Zone would be provided on the road frontage with a site crane to unload trucks.

Further details on construction access, operational site access and parking are located in the Traffic and Parking Impact Assessment prepared by Transport and Traffic Planning Associates in **Appendix Q**.

4.8 Safety and Security

The following section outlines CPTED principles and uses these to assess the architectural plans for the proposed redevelopment. This assessment has been undertaken in accordance with the NSW Government's CPTED principles (outlined below), and the requirements of Fairfield DCP 2013. The following section also outlines recommended mitigation measures to be considered in the final design.

There are a number of criteria to be considered when assessing Crime Prevention through Environmental Design (CPTED) as part of a development application. As stated by the NSW Government, CPTED aims to influence the design and management of buildings and places by:

- *Increasing the perception of risk to criminals by increasing the possibility of detection, challenge and capture.*
- *Increasing the effort required to commit crime by increasing the time, energy or resources which need to be expended.*
- *Reducing the potential rewards of crime by minimising, removing or concealing 'crime benefits'.*
- *Removing conditions that create confusion about required norms of behaviour.*

To that end, the four key principles to minimise the opportunity for Crime are outlined in the table below:

Table 4: CPTED Principles	
Principle	Definition
1. Natural Surveillance	Involves maximising opportunities for passers-by or residents to observe what happens in an area (the 'safety in numbers' concept). This highlights the importance of building layout, orientation and location; the strategic use of design; landscaping and lighting. Natural surveillance is a by-product of well-planned, well-designed and well-used space. Higher risk locations can also benefit from organised surveillance, which involves the introduction of formal measures such as on-site security guards or CCTV.
2. Access Control	Control of who enters an area so that unauthorised people are excluded, for instance, via physical barriers such as fences, grills etc.
3. Territorial Reinforcement/Ownership	People are more likely to protect territory they feel they own and have a certain respect for the territory of others. This can be expressed through installation of fences, paving, signs, good maintenance and landscaping. Territoriality relates to the way in which a community has ownership over a space.
4. Space Management	Ensures that space is appropriately utilised and cared for. Space management strategies include: activity coordination (i.e. having a specific plan for the way different types of activities are carried out in space), site cleanliness, rapid repair of vandalism and graffiti, the replacement of burned out lighting and the removal or refurbishment of decayed physical elements.

*Source: Crime Prevention & the Assessment of Development Applications
NSW Government Department of Planning*

Car Parking Areas

Car parking areas are typically common spaces for offences against property or persons. Relevant CPTED considerations for car parks include: access control, providing visibility, ensuring safe access for cars and pedestrians, and discouraging loitering. The subject DA proposes the following measures within the proposed basement level of the development:

- Adequate lighting compliant with AS1158 will be provided throughout, including the car park entry/exit points and loading/unloading areas.
- Clear demarcation of pedestrian walkways throughout the car park to avoid conflicts with vehicles will be provided through line marking and signage.
- Safe parking signage throughout car park to remind people to secure their cars and valuables will be installed prior to the occupation of the development.

Entry & Exit Points

Entry and exit points are a key consideration for CPTED assessments because of the interface they provide with the surrounding area. Relevant CPTED considerations for entry and exit points include: access control, providing visibility, ensuring safe access for cars and pedestrians, and discouraging loitering. The amended development application will maintain the following safety measures as originally proposed:

- Adequate lighting at all entry and entry points will be provided in accordance with the relevant Australian Standards.
- The entry/exit point is aesthetic and inviting and the landscaping, building position and activities are all orientated to maximise natural surveillance on entry/exist.

Internal Layouts

Key internal layout considerations in regard to CPTED include interfaces between public and private space; and the existence of 'dead space', 'areas of entrapment' and 'areas of concealment'. The revised application maintains the following measures:

- The design of new internal spaces does not create 'areas of entrapment or concealment, particularly in lobby, lift areas and access ways.
- All unit entries have good sight lines, are well lit and are in areas with ample room for access.
- Clear definition between public and private access areas are created using signage and secure doors.

External Areas

Key external layout considerations in regard to CPTED include visibility, activation and prevention of vandalism and graffiti. The amended DA maintains the following measures:

- All external areas of the building are proposed to be well lit, particularly at night.

- No opportunities for concealment are provided by external building design or landscaping based on the design of the proposal.

As identified above, the amended proposal has been designed with regard to the principles of Crime Prevention Through Environmental Design. Landscaped areas within the development site will be maintained to a good standard. Clear delineation between public and private areas is proposed. Lighting that complies with AS-1158 will be used. Car parking areas will be well lit at night and are in easy walking distance of the building.

The proposed development is therefore considered compliant with the requirements of CPTED.

4.9 Social and Economic Impact

The proposal will bring with it a number of important social and economic benefits for the local and wider community as outlined below:

- Enable Fairfield Council to continue to meet the needs of local residents through the provision of mix of affordable unit/room types and commercial space in a key location;
- Promote enhanced neighbourhood safety and security through casual surveillance generated by the presence of additional activity within the site;
- Provide short-term economic benefits through construction expenditure and employment; and
- Provide improvements to streetscape appearance, character, amenity of the area thus enhancing resident quality of life and satisfaction.

4.10 Site Suitability

Having regard to the characteristics of the site and its location in Fairfield, the development is considered to be appropriate having regard to the following elements:

- The zoning of the site permits the proposed use and the development has been designed having regard to the prescriptive measures to achieve the desired character as required by the Fairfield LEP and DCP;
- The size and dimensions of the land are appropriate for accommodating the proposal in its current form;
- The proposed development has been designed to allow for future development on surrounding lots and to protect the significance of the heritage building on site;
- It provides a well-designed built form on a site envisaged for high density development within walking distance to various public transport options;
- It is generally consistent with the site-specific objectives and provisions of the ARH SEPP, the Fairfield LEP 2013 and Fairfield DCP 2013; and
- The development is sited and designed to ensure no adverse impacts occur on surrounding development including overshadowing, noise and traffic impacts.

4.11 Public Interest

The proposal will facilitate the development of the site by providing a mixed-use development that meets the growing needs of the local and wider population. It is in the public interest to reinforce the importance of this location as a suitable development site for boarding house residential accommodation and commercial space. Generally, the proposal provides the following public benefits:

- It is consistent with the objects of the EP&A Act of encouraging the economic and orderly development of land;
- It has been carefully designed to ensure consistency with the applicable Council policies where possible;
- It provides a development with no adverse overshadowing impacts, noise impacts, traffic impacts or visual impacts; and
- Delivers a development that enhances and responds sensitively to its setting through the creation of a space that reflects the required scale and significance of the desired future context of the immediate precinct.

5.0 Conclusion

This proposal seeks approval for the repair and restoration works to an existing fire station and the construction of a 4-storey boarding house development at 3 William Street, Fairfield.

Consideration has been given to feedback received by Council I and the proposal is generally consistent with the relevant environmental planning instruments, including the ARH SEPP, the Fairfield LEP 2013, and Fairfield DCP 2013.

The proposal will directly contribute to the provision of a well-designed mixed-use development in the Fairfield City Centre. The proposed development has significant planning merit in the following respects:

- The proposed development provides additional housing to meet the needs of a growing population;
- The development is located on close proximity to range of services and amenities and public transport;
- Ensures the upkeep and restoration of a local heritage item and its reuse; and
- There are no adverse impacts on surrounding properties or the existing heritage item on site.

Having regard to the above, and in light of the relevant heads of consideration listed under Section 4.15 of the *Environmental Planning and Assessment Act, 1979* the proposal is reasonable and appropriate and warrants favourable consideration.



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